

PRIMAVERA FOUNDATION

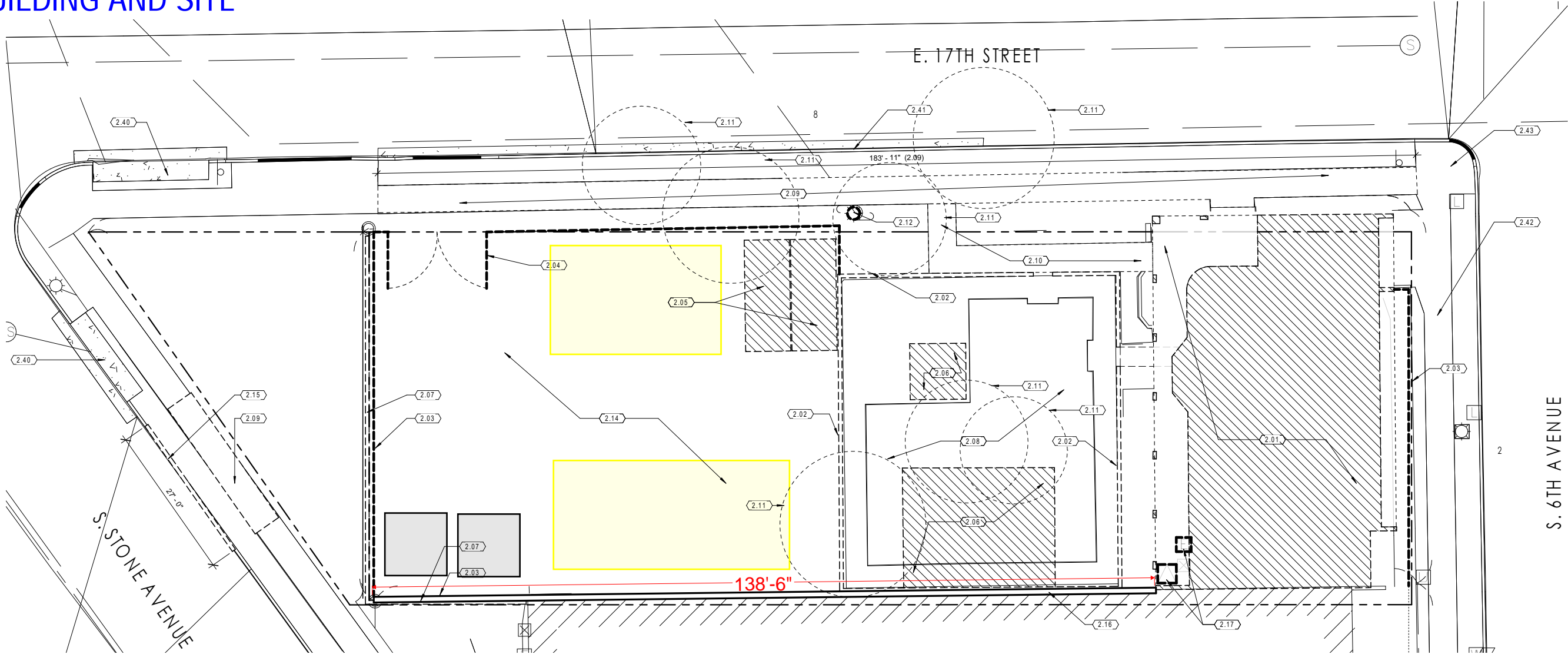
EMERGENCY SERVICES CENTER
BOARD OF ADJUSTMENTS PRESENTATION
6.24.20



SITE CONTEXT



EXISTING BUILDING AND SITE



ARCHITECTURAL SITE PLAN FROM CONSTRUCTION DOCUMENTS

NOTE: ENTIRE PARCEL IS CONSIDERED "DEVELOPING" AS THE PARCEL CONNECTED TO MS&R ROADS. PER COTDSD, BOTH 6TH AND STONE ARE FULLY DEVELOPED AT THIS LOCATION AND WILL NEVER BE INCREASED IN WIDTH SO FUTURE ROW NEED NOT BE CONSIDERED IN ASSIGNING SETBACKS BUT THE "DEVELOPING" BENEFIT OF BEING ON A MS&R LISTED ROAD IS STILL IN EFFECT.

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POSTER
MIRTO
McDONALD

PMM

ARCHITECTURE
PLANNING
PRESERVATION

317 N. COURT AVE, TUCSON, AZ 85701
PH 520.682.6310 | WWW.PMM.DESIGN

PARKING CALCULATION

REQUIRED PARKING:
10,210 SF @ 300 GSF / SPACE 34.03 =
35 REQUIRED PARKING SPACES

NOTE THAT THERE ARE 17 PLANNED EMPLOYEES FOR THIS FACILITY.

PROVIDED PARKING:
ON-SITE: 7 PARKING SPACES
PARALLEL, 17TH: 8 PARKING SPACES
PARALLEL, 6TH: 2 PARKING SPACES
PARALLEL, STONE: 2 PARKING SPACES

TOTAL PROVIDED: 19 PARKING SPACES

ALL PARALLEL ARE ON SAME SIDE OF STREET

CALCULATION OF SETBACKS:

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-AT PORTION CLOSEST TO STREET, BUILDING HEIGHT IS 17'-0" AND ACTUAL SETBACK IS 43'-0"

SOUTH PROPERTY:
REQUIREMENT IS 5' SEPARATION.
ACTUAL MINIMUM SEPARATION IS 5'-1"

KEYNOTES

- 3.03
3.08
3.09
3.10
3.11
3.12
4.02
4.03
5.10
5.12
5.22
5.23
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6.16
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32.01
- CONCRETE STAIRS
CONCRETE SONOTUBE SEATING - SEE LANDSCAPE
CONCRETE PATIO PAVING WITH 24" X 24" SAWCUT JOINTS
CONCRETE SITE STAIR DOWN TO BUILDING FFE FROM SIDEWALK LEVEL
CONCRETE SITE STAIR DOWN TO PAAL ELEVATION FROM BUILDING FFE
CONCRETE FOOTING FOR ROLLING GATE ANGLE TRACK
8X8X16 CMU SITE WALL WITH RAKED HORIZONTAL JOINTS AND FLUSH VERTICALS - RUNNING BOND - PAINTED - 4" SOLID CAP BLOCK
MASONRY RETAINING WALL - SEE CIVIL
STEEL GUARDRAIL AND/OR HANDRAIL SYSTEM WITH PAINTED FINISH
STEEL GATE PER DETAILS
VEHICULAR ROLLING GATE - 4X4 TUBESTEEL FRAME WITH 2X4 VERTICALS 4" O.C. INFILLED WITH 2 X 2 X 1/4" WWF
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6'-0" HIGH STEEL PICKET FENCE - 1X1 TUBE STEEL PICKETS WITH 3" TUBE VERTICALS 6" O.C. PAINTED. ON TOP OF MASONRY WALL
WALL BETWEEN THIS BUILDING AND EXISTING BUILDING TO SOUTH - HOLD 4' AWAY FROM EXISTING BUILDING
BICYCLE LOCKER
BIKE RACK
WALL MOUNTED BIKE RACK
SLIDING GATE OPERATOR EQUAL OF HY-SECURITY CNX - PROVIDE LEADING EDGE PROTECTION AND PHOTOSENSORS - PROVIDE CONDUIT CONNECTIONS TO PHOTOSENSORS, POWER SOURCE
SERVICE ENTRANCE SECTION
PLANTING AREA - SEE LANDSCAPE



PRIMAVERA FOUNDATION
EMERGENCY SERVICES CENTER

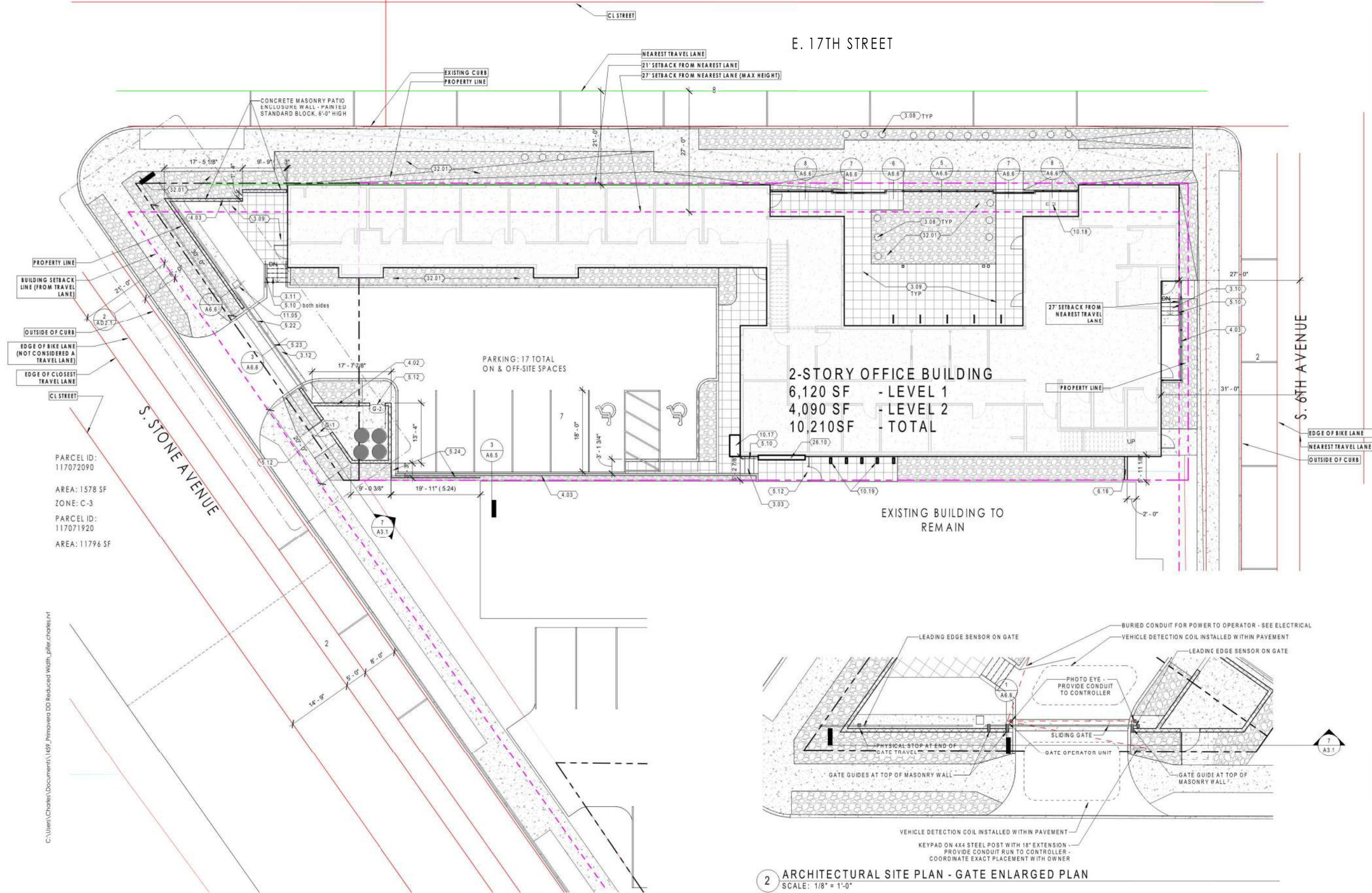
702 S. SIXTH AVENUE, TUCSON AZ 85701

AD2.1 ARCHITECTURAL SITE PLAN

DRAWN BY: CJP
CHECKED BY: SGM

REVISIONS
NO. DATE ISSUE

© DATE: 3.9.20
PROJECT NO: 1459



2 ARCHITECTURAL SITE PLAN - GATE ENLARGED PLAN
SCALE: 1/8" = 1'-0"

1 ARCHITECTURAL SITE PLAN
SCALE: 1" = 10'-0"

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ARCHITECTURAL SITE PLAN FROM CONSTRUCTION DOCUMENTS

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3.08	CONCRETE SONOTUBE SEATING - SEE LANDSCAPE
3.09	CONCRETE PATIO PAVING WITH 24" X 24" SAWCUT JOINTS
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3.12	CONCRETE FOOTING FOR ROLLING GATE ANGLE TRACK
4.02	8X8X16 CMU SITE WALL WITH RAKED HORIZONTAL JOINTS AND FLUSH VERTICALS - RUNNING BOND - PAINTED - 4" SOLID CAP BLOCK
4.03	MASONRY RETAINING WALL - SEE CIVIL
5.10	STEEL GUARDRAIL AND/OR HANDRAIL SYSTEM WITH PAINTED FINISH
5.12	STEEL GATE PER DETAILS
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11.05	SLIDING GATE OPERATOR EQUAL OF HY-SECURITY CNX - PROVIDE LEADING EDGE PROTECTION AND PHOTOSENSORS - PROVIDE CONDUIT CONNECTIONS TO PHOTOSENSORS, POWER SOURCE
26.10	SERVICE ENTRANCE SECTION
32.01	PLANTING AREA - SEE LANDSCAPE

REGISTERED ARCHITECT
S1665
SAVANNAH G.
McDONALD
DATE SIGNED
12.31.22
EXPIRES: 12.31.22

PRIMAVERA FOUNDATION
EMERGENCY SERVICES CENTER

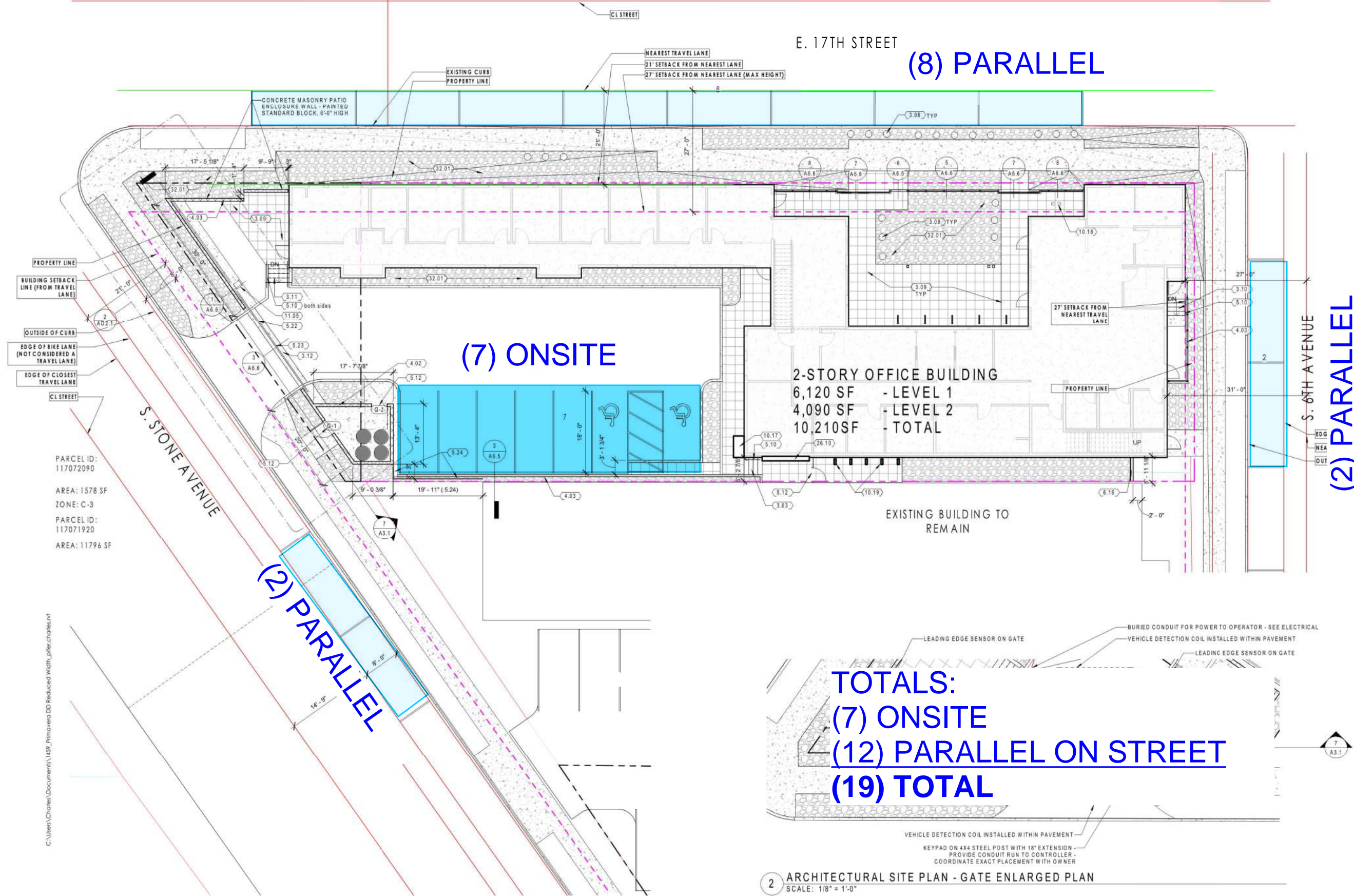
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2 ARCHITECTURAL SITE PLAN - GATE ENLARGED PLAN
SCALE: 1/8" = 1'-0"

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Alternative Transportation Incentive Plan (ATIP)

A. PURPOSE AND SCOPE

Primavera Foundation (Primavera) offers an Alternative Transportation Incentive Plan (ATIP) designed to reduce the number of single occupant vehicles (SOV) driven to work. This reduces parking congestion at our facilities and helps protect the air quality in our community.

B. GENERAL GUIDELINES

The incentive program provides cash awards, and preferential parking for those who carpool, to staff members who use an alternative form of transportation to get to work.

Alternative Transportation Methods (ATMs)

For the purposes of ATIP alternative transportation shall include the following:

- Carpooling
- Public transportation
- Bicycling or similar
- Walking

Eligibility

All full and part time regular staff are eligible for the incentive. Temporary staff is not eligible unless they are working a long term assignment at a Primavera facility. All eligible staff must register with the Finance Department and receive their ATIP Log in order to receive incentives from the program.

Incentives

Staff members may earn up to \$300 annually. The incentives are earned on a monthly basis and paid out quarterly. To earn a monthly incentive of \$25, the staff member must use ATMs, as listed above, on at least 12 business days of the month (with the exception of 11 days in February). Business days are defined as any day that the Primavera facility is open for regular business and during regular business hours.

Additionally, those who car pool will receive guaranteed, preferential parking.

ATIP Log

In order to earn incentives, staff members must use ATMs at least 12 days per months. The staff member should track this on their ATIP Log and turn the log in quarterly. The Staff member's Supervisor and Senior Leader must sign off on the form each quarter. Up to \$75 will be paid each quarter for an annual possible incentive of \$300. Per federal and state tax statutes, these incentives are taxable.

Guaranteed Ride Home

Guaranteed Ride Home provides an emergency taxi or other ride hailing service ride home for employees who commute to work using an alternative to driving alone and have an illness or emergency. Primavera will pay for the ride hailing service of your choice.

C. FRAUD AND ABUSE OF ATIP

Staff members who submit false ATIP logs or claim incentives they did not earn are subject to losing their incentive privileges and disciplinary action, up to and including termination, depending on the severity of the abuse.

Parking Reduction Variance:
UDC SECTION REFERENCE:

7.4.3. GENERAL PROVISIONS

A. Parking Required

1. Parking is required for every use unless otherwise specified in Section 7.4.4.B, Minimum Number of Motor Vehicle Spaces Required, or 7.4.8.B, Minimum Number of Bicycle Parking Spaces Required.

F. Calculation of Required Motor Vehicle and Bicycle Parking Spaces

The minimum number of parking spaces required must be calculated based on the particular characteristics of the use. The following methods shall be used to calculate the required number of parking spaces.

4. Based on Gross Floor Area (GFA)

Use the total GFA of all applicable land uses within the development site, plus the area of any outdoor areas necessary to provide the service to the public or conduct the activity, such as outdoor eating areas or outdoor areas used for sale of merchandise, to calculate the requirement. The calculation does not include vehicular use areas, automobile display areas, or other outdoor areas used for nonpublic purposes. Where such areas are identified on a development plan but are not defined, the Zoning Administrator shall determine the extent of the area.

TABLE 7.4.4-1: MINIMUM NUMBER OF MOTOR VEHICLE SPACES REQUIRED
COMMERCIAL SERVICES USE GROUP: 1 space per 300 sq. ft. GFA

Requirement: 10,210 GSF building / 300SF= 34.03 on-site spaces required
Providing: 7 on-site spaces provided. Adjacent street parking will be utilized if needed.
Note, the IBC required (2) accessible parking spaces are provided on-site.

Landscape Border Variance:

UDC SECTION REFERENCE:

7.6.4. LANDSCAPE STANDARDS

2. Street Landscape Borders

To enhance the visual appearance of the streetscape, a street landscape border is required in accordance with Table 7.6.4-1 along the street frontage of a site as follows, except as provided in Section 7.6.4.C.4, Exceptions to the Landscape Border Standards:

a. Minimum Width

Street landscape borders must be a minimum of ten feet wide as measured from the street property line. On streets designated as Major Streets and Routes (MS&R), the street landscape border must be measured from the future MS&R right-of-way line as determined by Section 5.4.4, Establishment of MS&R Right-of-Way Lines and Gateway Routes.

c. Located on Site

Street landscape borders must be located entirely on site, except that, if approved by the City Engineer or designee, up to five feet of the required ten foot width may be placed within the adjacent right-of-way area or within the Major Streets and Routes (MS&R) right-of-way area on MS&R streets.

Requirement: 10' landscape border within the property measured from property line.

Request: To modify and/or delete the street landscape border as shown on the attached plans.

Note that we are proposing providing a 10'+ landscape border including landscape within right of way.

PROJECT INFORMATION

- The project will demolish the existing two-story Emergency Services Center building located on parcel 117-07-1920, and replace it with a new 2-story, 10,210 square foot office building with a maximum height of 26'-5". The first-floor footprint will be 6,120 SF.
- The combined lot square footage is 13,374 SF (11,796 for 117-07-1920 + 1,578 for 117-07-2090).
- The proposed building is designed to meet all applicable building and zoning codes save for the two specific variances here requested.

Landscape Buffer:

- Odd shaped lot imposes the landscape border requirement along a longer than average percentage of the parcel perimeter. The landscape border per code requires 26% of the site area.
- The project will install a considerably greater quantity of landscaping than currently exists on the parcel, and will provide the equivalent 10' worth of landscape around the entire perimeter, though some will be within the right of way rather than on the property.
- The proposed variance is the minimum possible modification.

Parking Variance:

- Given the constraints of the site, every effort was taken to maximize efficiency of the building within budget, and to maximize parking on-site. Primavera foundation is undertaking a company program to curtail use of personal automobiles by their employees which will further reduce the amount of parking which is required by this building.

Summary:

- The proposed development of the subject parcel is not out of character with the neighborhood.
- Compliant buffer and parking when taken together render the parcel undevelopable.
- Urban design was carefully considered - maintains prevailing setbacks for the block and prevents a corner from being developed as parking.

ARCHITECTURAL SITE PLAN FROM CONSTRUCTION DOCUMENTS

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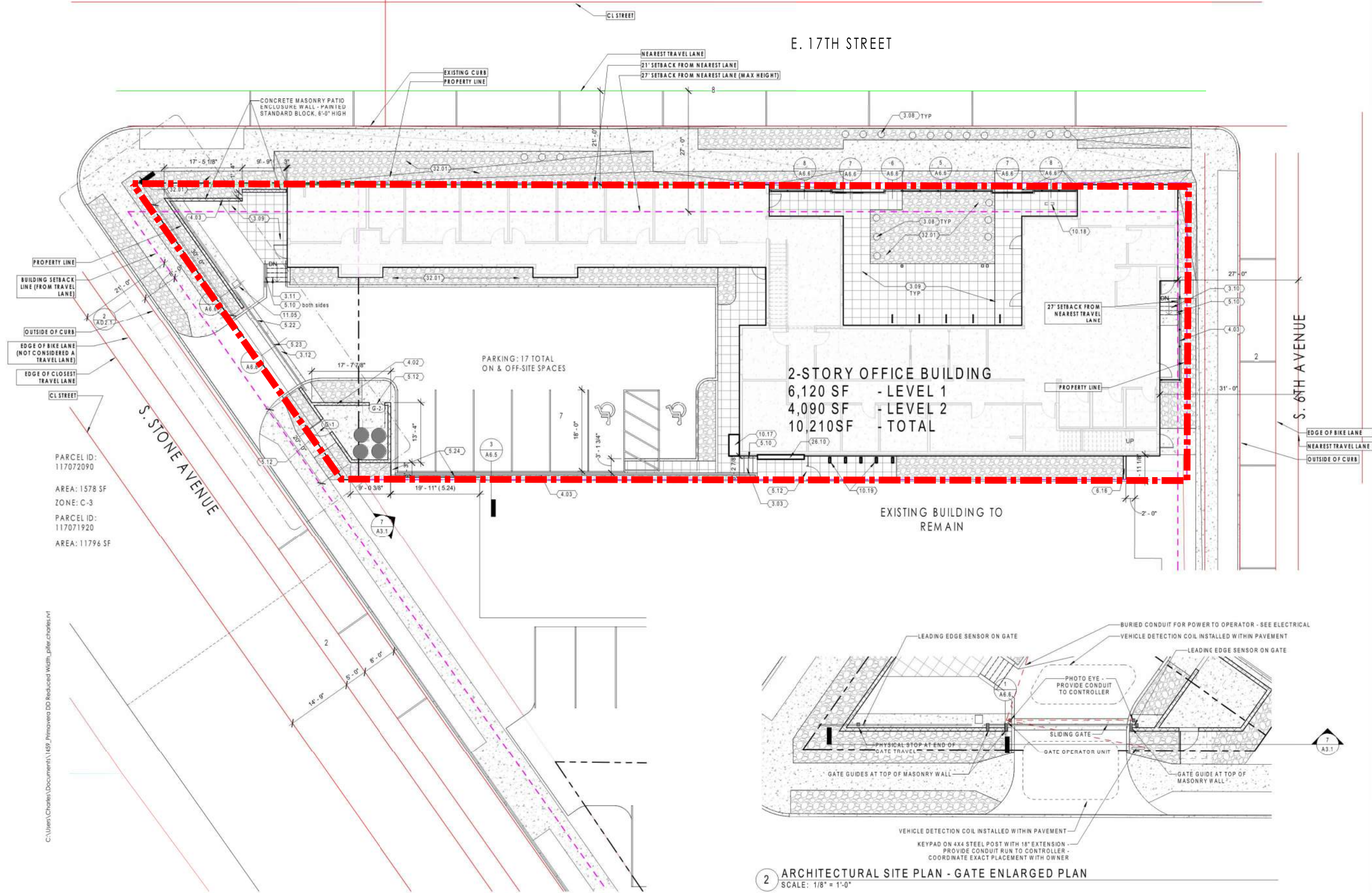
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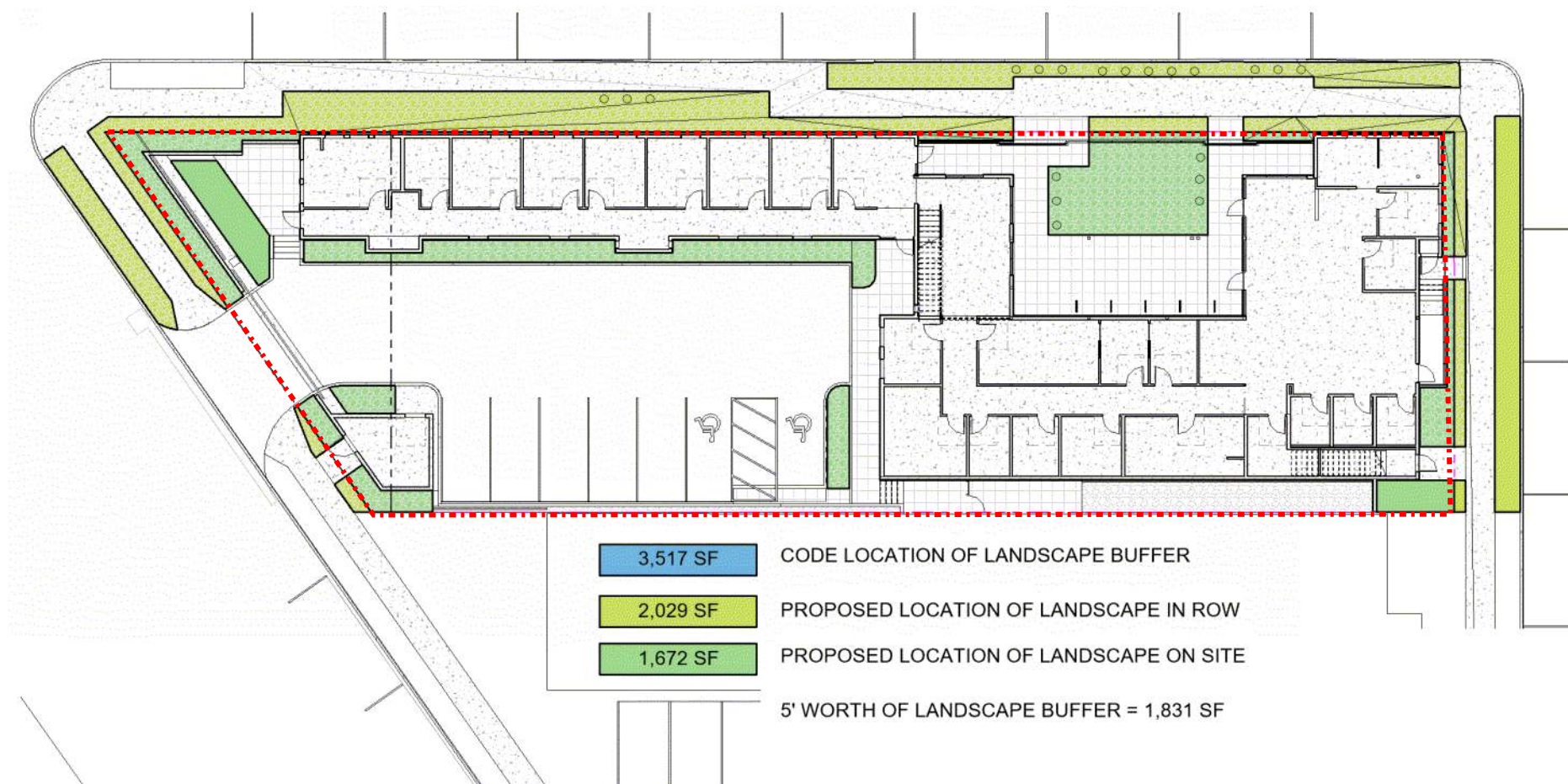
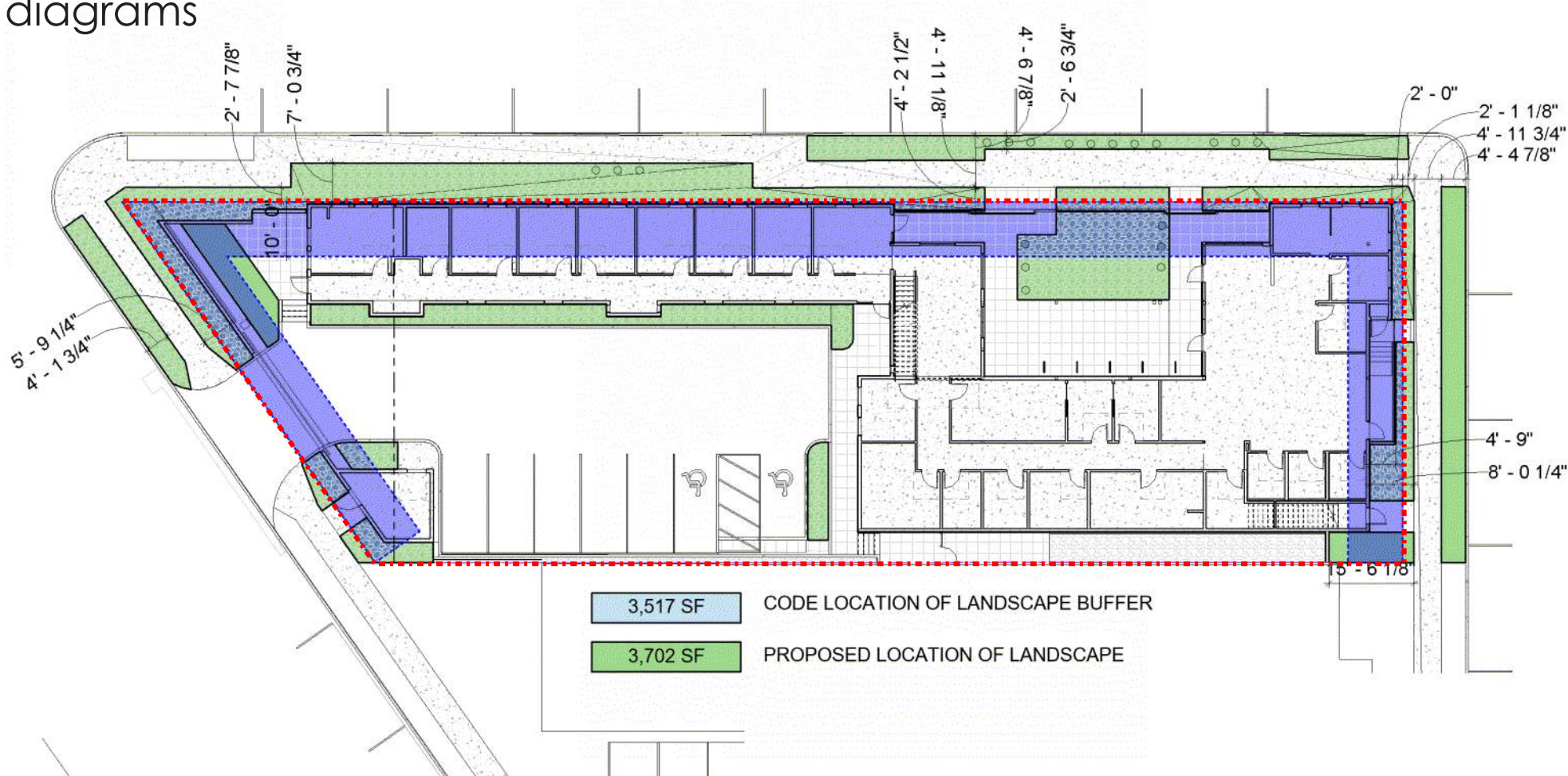
2 ARCHITECTURAL SITE PLAN - GATE ENLARGED PLAN
SCALE: 1/8" = 1'-0"

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SCALE: 1" = 10'-0"

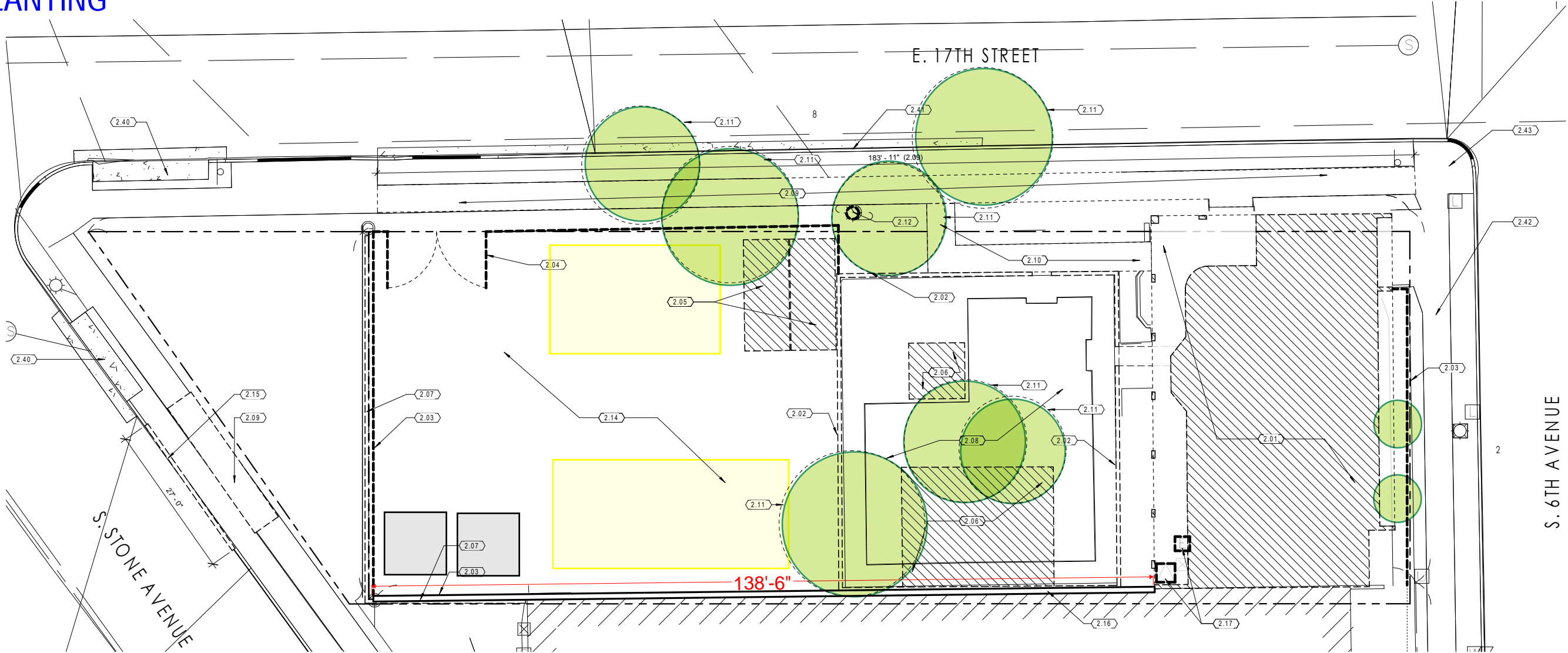
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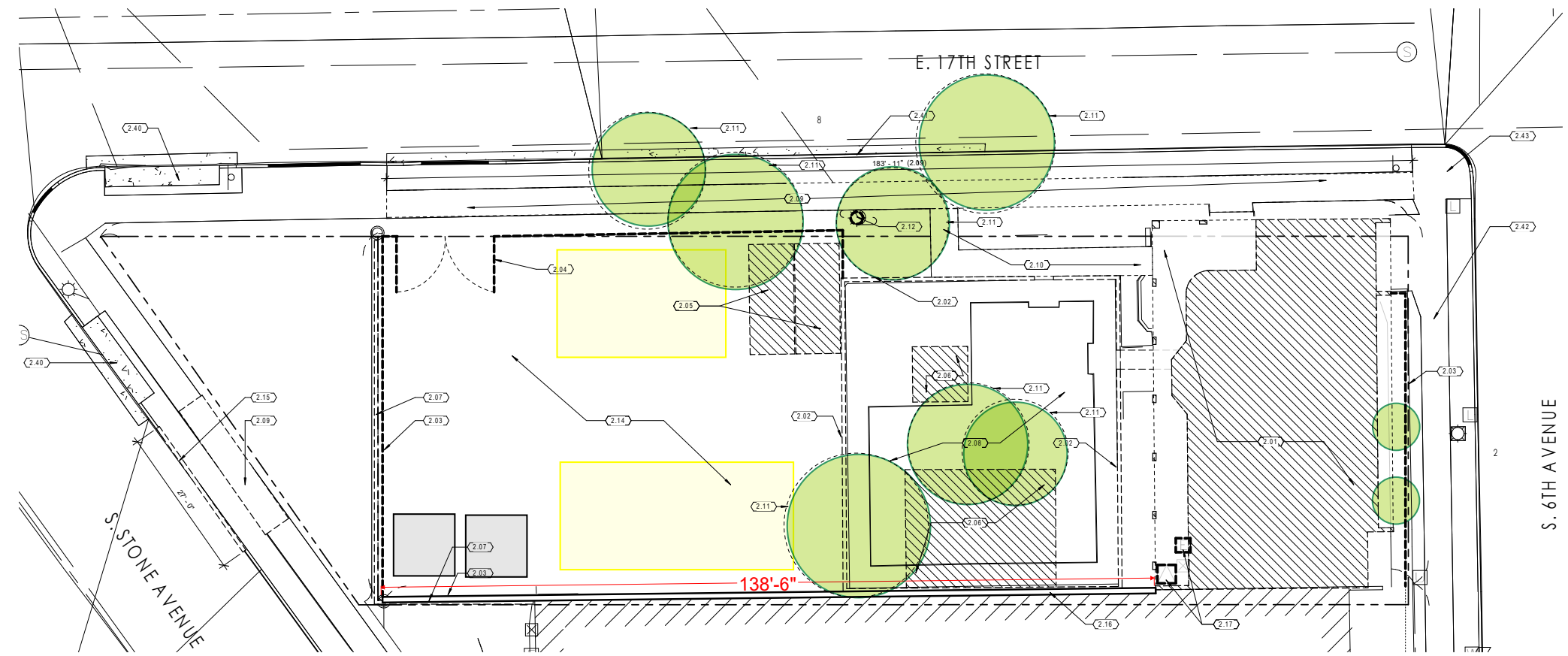
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Landscape buffer diagrams

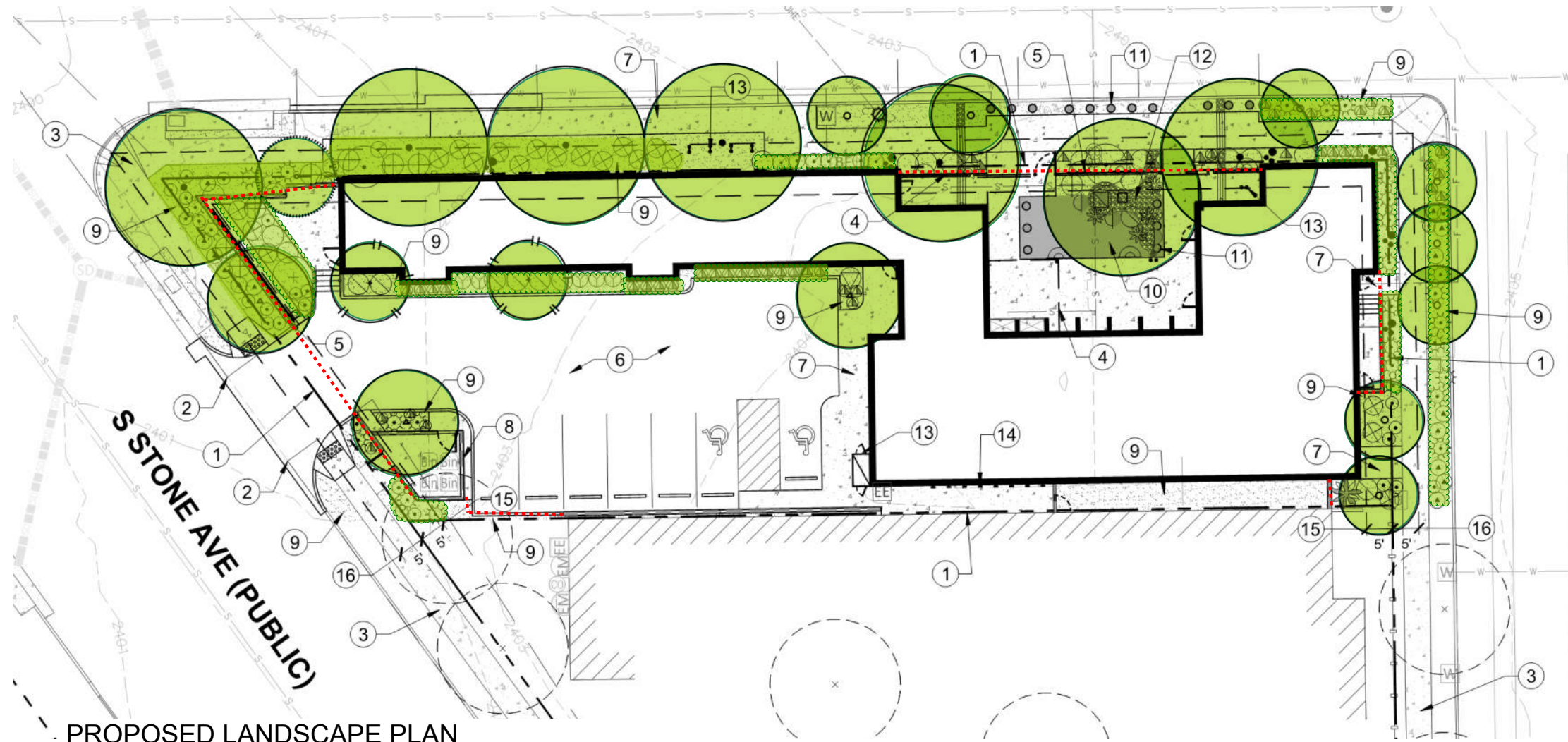


EXISTING PLANTING



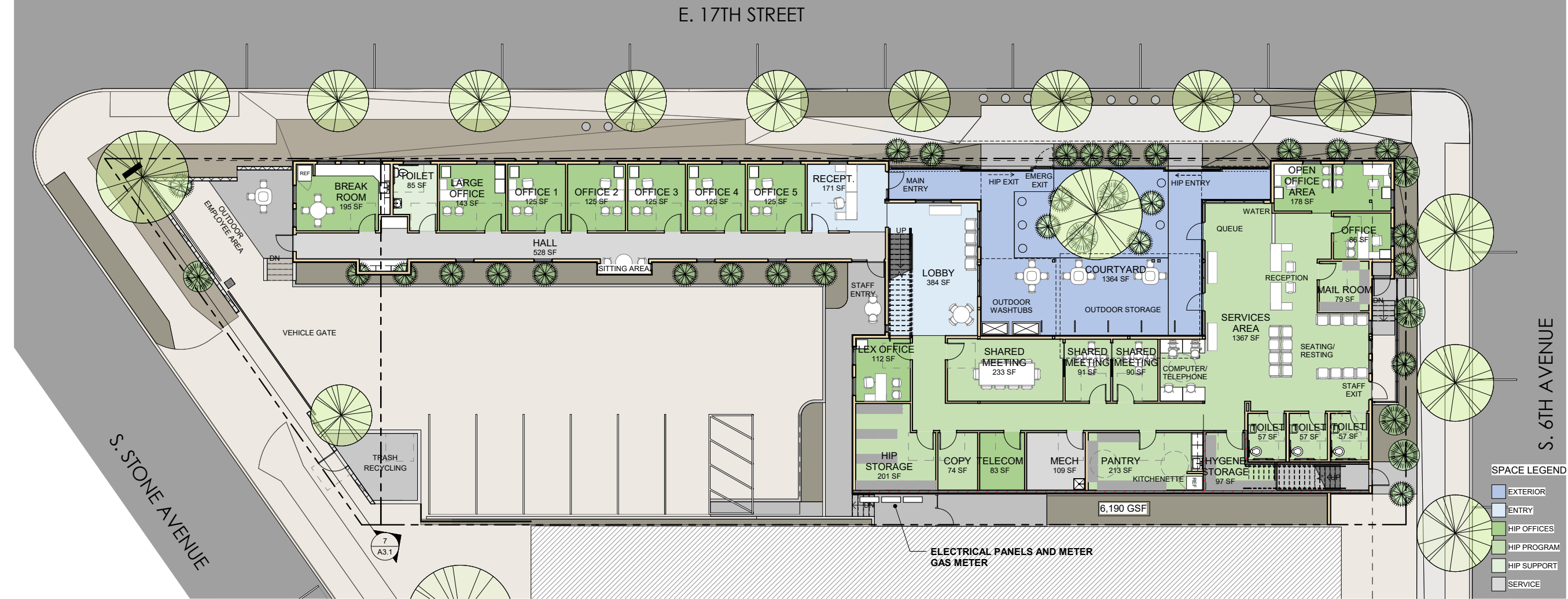


EXISTING LANDSCAPE PLAN

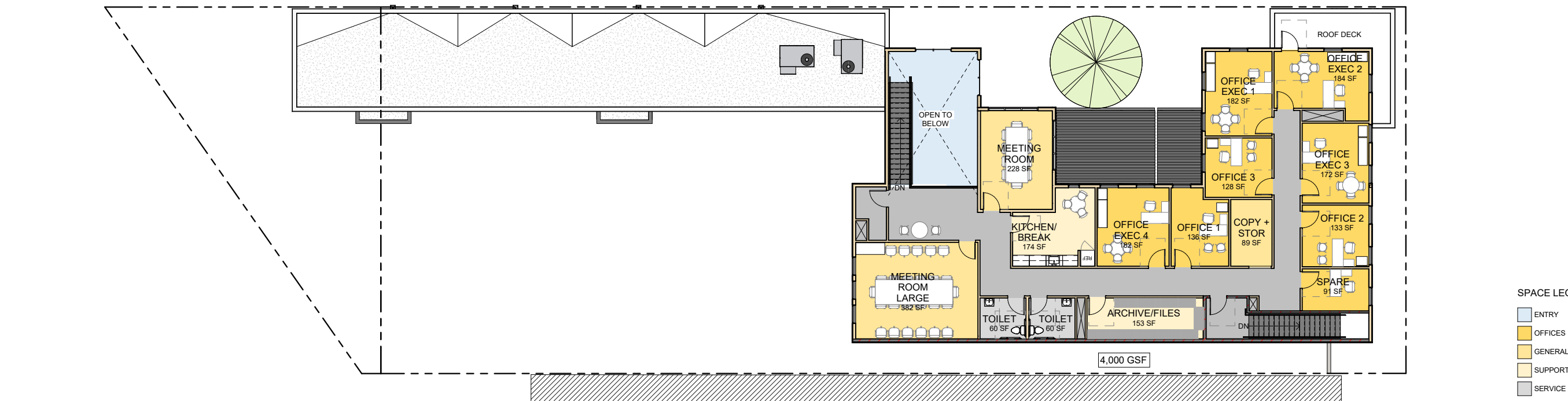


PROPOSED LANDSCAPE PLAN

END OF PRESENTATION



1 LEVEL 1 FLOOR PLAN PRESENTATION
SCALE: 1" = 20'-0"

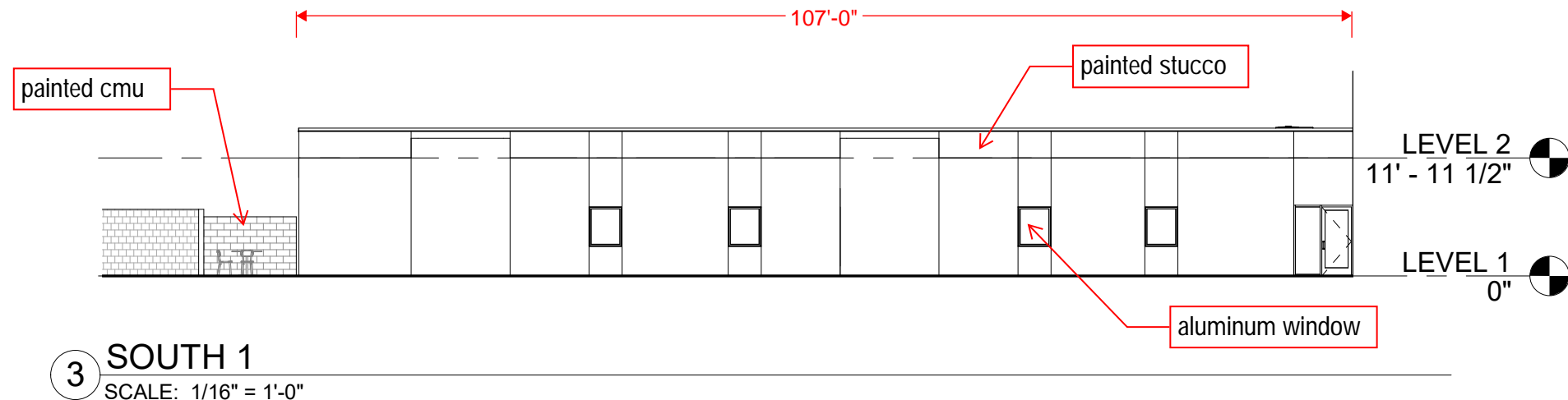
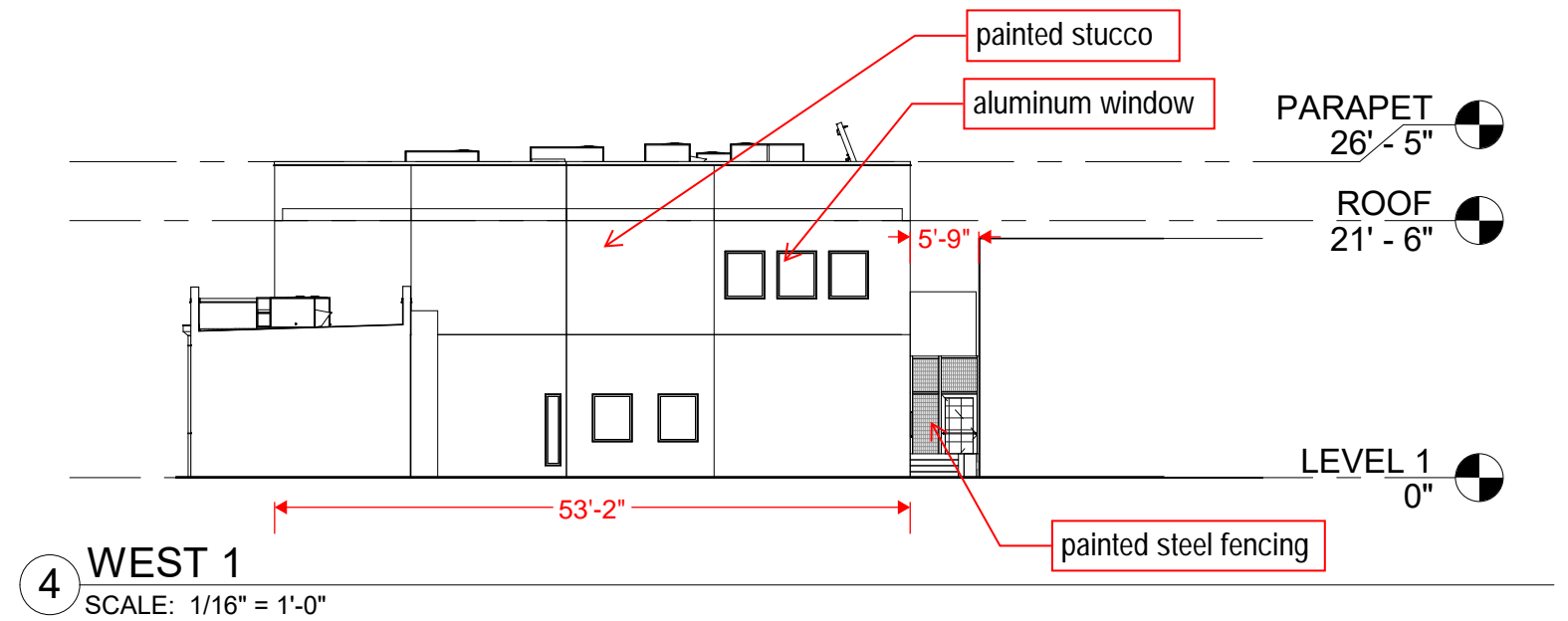
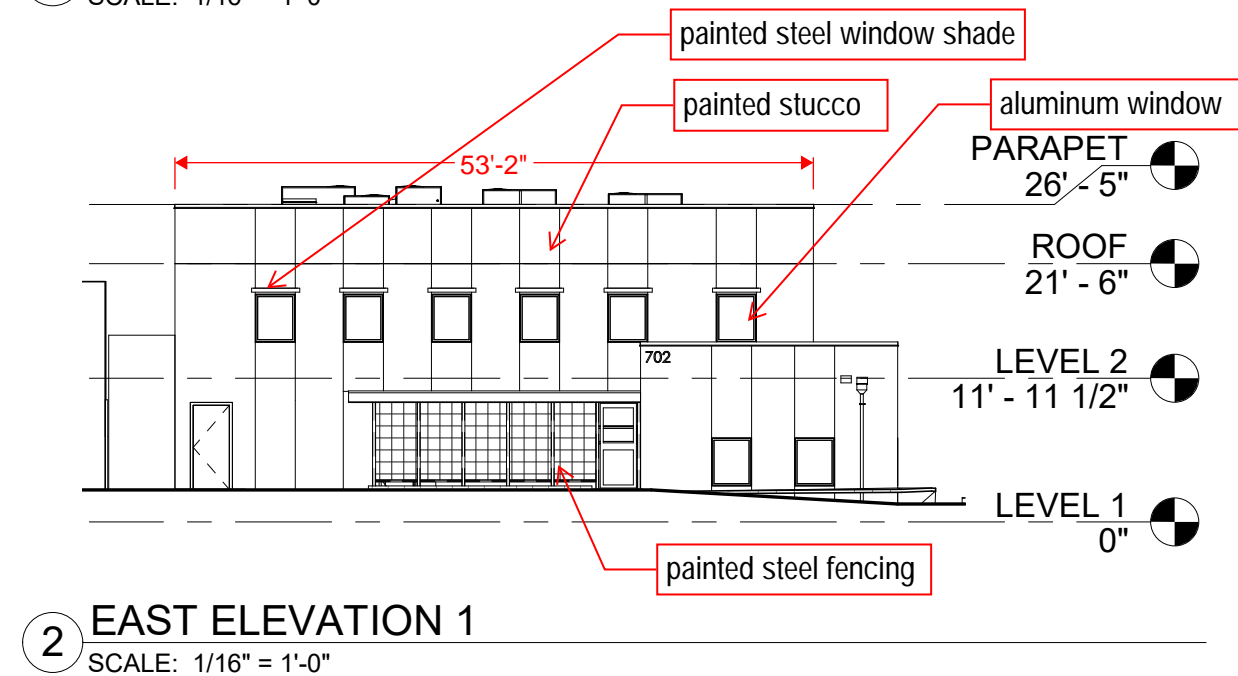
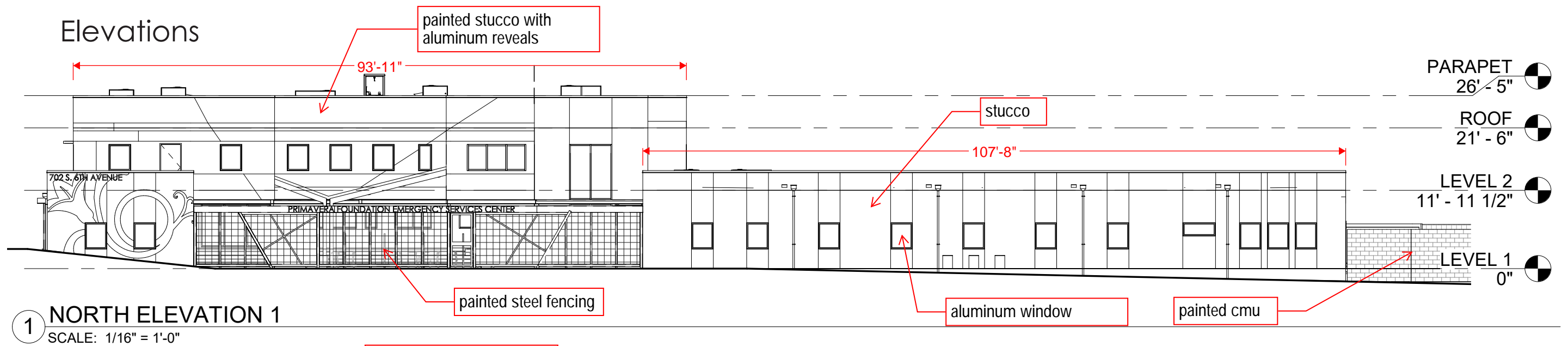


2 LEVEL 2 FLOOR PLAN PRESENTATION
SCALE: 1" = 20'-0"





Elevations





3 A - NW CORNER VIEW
SCALE:



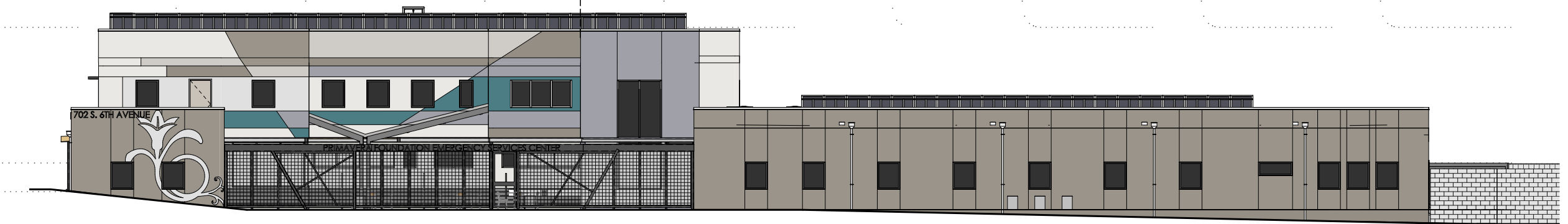
1 A - NE CORNER VIEW
NOT TO SCALE



4 A - COURTYARD VIEW
SCALE:



2 A - COURTYARD FROM STREET
NOT TO SCALE



5 NORTH PRESENTATION ELEVATION
SCALE:

NOT FOR
CONSTRUCTION

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EMERGENCY SERVICES CENTER

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PRES1.2 PRESENTATION
PERSPECTIVES

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